



Town of Christiansburg Planning Commission Monday, December 15, 2025 Agenda

Planning Commission

Chairperson

Jeananne Knees

Vice-Chairperson

Felix Clarke

Other Members

Mark Curtis

Larry Day Jr.

Hil Johnson

Helen Lyman

Mike Scarry

Jennifer Sowers

Planning Commission Meeting

Next Meeting:

Monday, December
29, 2025 at 7:00 p.m.

Town Council Meeting

Public Hearing:

Tuesday, January 13,
2026 at 7:00 p.m.

Action:

*Tentatively
Scheduled*

Tuesday, January 27,
2026 at 7:00 p.m.

REGULAR MEETING

Planning Commission will meet in the Town Council Chambers on the 3rd floor of Christiansburg Town Hall located at 100 E. Main Street on **Monday, December 15, 2025 at 7:00 p.m.**

- 1) Pledge of Allegiance
- 2) Public Comments – 5-minute limit per citizen
- 3) Approval of Planning Commission Minutes for December 1, 2025.
- 4) Discussion and Action
 - A. Conditional Use Permit request by Clayco, LLC, Property Owner and Applicant, to allow a Planned Housing Development of 28 multi-family residential dwelling units on approximately 2.031 acres located northwest of the intersection of Chrisman Street SW and First Street SW, between 101 Chrisman Street SW and 400 W Main Street; consisting of all of Tax Parcel 526-((A))-138 (Parcel ID 013446); zoned Multifamily Residential District (R-3). The property is designated as Residential on the Future Land Use Map of the 2013 Christiansburg Comprehensive Plan.
- 5) Other Business

A copy of the applications, the Town's Zoning Map, Zoning Ordinance, and Future Land Use Map may be viewed in the Planning Department Office, 100 E. Main Street, Christiansburg, VA 24073 during normal office hours of 8:00 a.m. – 5:00 p.m. Monday through Friday. Contact the Planning Department, by phone at (540) 382-6120, ext. 1902, or by email at planning@christiansburg.org with any questions or if you require reasonable accommodations.

**Christiansburg Planning Commission
Minutes of December 1, 2025
Christiansburg Town Hall Council Chambers
100 E. Main Street
Christiansburg VA 24073**

Present: Jeananne Knies, Chairperson
Felix Clarke Jr., Vice-Chairperson
Mark Curtis
Larry Day Jr.
Hil Johnson
Helen Lyman
Mike Scarry
Jennifer Sowers

Absent:

Staff/Visitors: Retta Jackson, Planning Director
Devon Shields, Community Development Planner
Daria Chicosky, Property Maintenance/Zoning Supervisor
Jillian Layton, Planning Services Coordinator
Steve Semones, Balzer & Associates, 80 College Street, Christiansburg VA 24073
Ian Friend, SAS Builders, Inc., PO Box 10397, Blacksburg VA 24062
Ramah Steinruck, 121 Longview Drive, Christiansburg VA 24073
Kyle Brooks, 114 Longview Drive, Christiansburg VA 24073
Diane Gabriel, 116 Evans Street, Christiansburg VA 24073
Reuel Edson, 101 Angle Drive, Christiansburg VA 24073
Olivia Brooks, 114 Longview Drive, Christiansburg VA 24073

Chairperson Knies called the meeting to order at 7:00 p.m. in the Christiansburg Town Hall Council Chambers at 100 E. Main Street, Christiansburg, Virginia.

Pledge of Allegiance

Commissioner Day led the Pledge of Allegiance.

Public Comment

Chairperson Knies opened the floor for public comment.

Seeing no public comments, Chairperson Knies closed this portion of the meeting.

Approval of Planning Commission Minutes for the October 20, 2025, Meeting

Upon a motion by Commissioner Johnson, seconded by Commissioner Curtis, the October 20, 2025, Planning Commission meeting minutes were approved.

Commission voted as follows: Clarke – Aye; Curtis – Aye; Day – Aye; Johnson – Aye; Lyman – Aye; Scarry – Aye; Sowers – Aye; Kries – Aye.

A. **Public Hearing for Conditional Use Permit request by Clayco, LLC, Property Owner and Applicant, to allow a Planned Housing Development of 28 multi-family residential dwelling units on approximately 2.031 acres located northwest of the intersection of Chrisman Street SW and First Street SW, between 101 Chrisman Street SW and 400 W Main Street; consisting of all of Tax Parcel 526-((A))-138 (Parcel ID 013446); zoned Multifamily Residential District (R-3). The property is designated as Residential on the Future Land Use Map of the 2013 Christiansburg Comprehensive Plan.**

Chairperson Kries asked staff for a brief overview of the request.

Devon Shields, Community Development Planner, detailed the purpose of the request. She stated that the applicant was requesting deviations from the standard unit count and maximum height of the structure. She stated the project aligned with the Town's Comprehensive Plan and Future Land Use Map. She also detailed the staff recommended conditions.

Chairperson Kries opened the floor for public comment.

Steve Semones, Balzer and Associates, 80 College Street, Christiansburg VA 24073, introduced the developer partner of the proposal, Ian Friend, and stated he would be giving the Commission a quick background of the property.

Mr. Friend spoke about the project and stated that due to the increase of cost with the by-right development, and desire to keep rent to a reasonable price, they were requesting the CUP to be able to add a few more units for each proposed building.

Mr. Semones spoke more in detail about the specifics of the request and mentioned that by-right they had the ability to build 20 units. He spoke about the details of the topography of the property as well as the added parking spots to account for the increase in units. He mentioned a typo in the previously provided narrative in regard to setbacks and noted that these are primarily 1- and 2-bedroom units. He also detailed proposed stormwater management, parking, the one access point, fire hydrants, property management, water and sewer accessibility, sidewalks, and private dumpster locations. He then stated they were in agreement with the three staff recommended conditions.

Chairperson Kries clarified that the two deviations being requested were for an increase in units and the height of the building.

Ramah Steinruck, 121 Longview Drive, Christiansburg VA 24073, read a letter in opposition written from a neighbor who was unable to attend the Public Hearing. The letter highlighted concerns with the lack of greenspace, increased density, light pollution, hazardous drop at the stormwater retention pond, traffic, and minimal parking spots which would contribute to added street parking. Ms. Steinruck then added her own concerns about an increase in traffic on the current narrow roads.

Kyle Brooks, 114 Longview Drive, Christiansburg VA 24073, spoke in opposition of the request. He spoke about concerns with the narrowness of the current streets, increase in traffic, density, parking, blocking of the current view, and the lack of proposed actions that could be taken to mitigate the risks he was concerned about.

Diane Gabriel, 116 Evans Street, Christiansburg VA 24073, spoke in opposition of the request. She stated that she was concerned with the roads being so narrow, the lack of sidewalks, the safety of the children in the neighborhood, and traffic.

Reuel Edson, 101 Angle Drive, Christiansburg VA 24073, spoke in opposition of the request. He stated that he was concerned with the narrowness of the roads, traffic, and the potential of this being used as student housing.

Olivia Brooks, 114 Longview Drive, Christiansburg VA 24073, spoke about concerns with the increase in traffic, pedestrian safety, and the lack of sidewalks.

Seeing no more public comments, Chairperson Kries closed the public hearing.

Chairperson Kries asked the Commission for any questions or comments.

Commissioner Johnson asked Mr. Semones if there was a second access considered for the proposal.

Mr. Semones stated that they were not able to get a second access point primarily due to the grading of the property.

Commissioner Johnson asked for clarification on where the sidewalks would be put for this new development.

Commissioner Sowers spoke about the concerns that had been brought up regarding the width of the roads.

Chairperson Kries clarified that by-right they were able to build 20 units, and they were asking for approval to build 28 units.

The Commission spoke more in detail regarding the proposed increase in units, the increase in traffic, and anticipated rental rate per unit.

Commissioner Curtis asked staff about the possibility of having a throughway through the proposed development.

Mrs. Shields clarified that the area Commissioner Curtis was inquiring about was vacated right-of-way and now consisted of private parcels.

Commissioner Scarry stated "Pursuant to VA Code Section 2.2-3112 section B1 and 2.2-3115 section H, I am declaring that I am a member of one of three or more businesses that may be affected by the Agenda Items 4 and 5 as an owner of an

electrical and electronic security contractor. I am able to participate in the discussion and voting on this matter fairly, objectively, and in the public interest". He then asked staff if a planned housing development was by-right in this area.

Daria Chicosky, Property Maintenance/Zoning Supervisor, stated that a planned housing development required a Conditional Use Permit due to a higher requested density and other design variations.

There was further discussion regarding Planned Housing Developments.

Commissioner Scarry spoke about this neighborhood's proximity to Downtown and how he felt that encouraged people walking and biking to nearby areas.

Chairperson Knies stated she felt like the development was not in a location that necessarily encouraged students to live there but noted she believed it was a reasonable concern.

Commissioner Curtis asked if it was a requirement to have a second access point to the proposed development.

The Commission and staff discussed in detail potential access points.

Vice-Chairperson Clarke mentioned he liked the idea of the development but also had concerns with the access road being so narrow.

B. Discussion and Action for Plat showing dedication of Right-of-Way for Underwood Street NW in the vicinity of Tax Parcels 526 – (24)- 6A,7.

Chairperson Knies asked staff for an overview of the request.

Mrs. Shields spoke about the request and detailed why it was a requirement by Town Code.

Chairperson Knies opened the floor for public comment.

Seeing no public comments, Chairperson Knies closed the public hearing.

Upon a motion by Commissioner Johnson, seconded by Commissioner Scarry, the plat showing dedication of Right-of-Way for Underwood Street NW was tentatively approved.

Commission voted as follows: Clarke – Aye; Curtis – Aye; Day – Aye; Johnson – Aye; Lyman – Aye; Scarry – Aye; Sowers – Aye; Knies – Aye.

Other Business

Commissioner Johnson spoke about a meeting he would be attending for the I-81 Commission and stated he would share the information received at the next Planning

Planning Commission Minutes of December 1, 2025

Commission meeting.

Chairperson Knies mentioned upcoming Town events.

With no further business, Chairperson Knies adjourned the meeting at 7:59 pm.

Jeananne Knies, Chairperson

Retta Jackson, Secretary Non-Voting

* Written comments received on public hearing items are included in the staff report to Planning Commission in the Planning Commission agenda packet.



Planning Department Staff Report

TO: Planning Commission
DATE: December 12, 2025
PC PUBLIC HEARING: Monday, December 1, 2025, at 7 PM
PC RECOMMENDATION: Monday, December 15, 2025, at 7 PM*
*Scheduled recommendation
AGENDA ITEM: Conditional Use Permit 2025 - 06
Conditional Use Permit request by Clayco, LLC, Property Owner and Applicant, to allow a Planned Housing Development of 28 multi-family residential dwelling units on approximately 2.031 acres located northwest of the intersection of Chrisman Street SW and First Street SW, between 101 Chrisman Street SW and 400 W Main Street; consisting of all of Tax Parcel 526-((A))-138 (Parcel ID 013446); zoned Multifamily Residential District (R-3). The property is designated as Residential on the Future Land Use Map of the 2013 Christiansburg Comprehensive Plan.

Site Characteristics:

Address: Chrisman Street SW
Tax Parcel No.: 526-((A))-138
Parcel ID: 013446
Current Zoning: Multifamily Residential District (R-3)
Future Land Use: Residential

Background

The proposed Conditional Use Permit (CUP) consists of 2.031 acres located northwest of the intersection of Chrisman Street SW and First Street SW, identified as all of Tax Parcel 526-((A))-138 (Parcel ID 013446). The surrounding zoning designations include Central Business (B-2) to the north; Multifamily Residential (R-3) to the northwest, northeast, and east; and Single-Family Residential (R-1) to the south and west. Nearby property uses include townhomes, single-family residences, and the MCEAP retail space and donation center. Unimproved First Street and Angle Drive right-of-way adjacent to the property was vacated in 2023 to support development of the property.

Summary of Request

The applicant is requesting approval of a CUP to permit the use of a Planned Housing Development (PHD) with a proposal of 28 residential units on approximately 2.031 acres. Per the application, each building will have a mix of one-, two- and three-bedroom units. The buildings will be designed as three/four split story buildings with associated parking for residents.

PHDs within the Multifamily Residential District (R-3) have a maximum density requirement of 20 units per acre. The project's proposed density will be 13.79 units per acre. The maximum height of the building will be 45 ft. with no additional setbacks proposed. The proposed facility contains two parking spaces per unit, totaling 56 parking spaces.

The proposed development aligns with most multi-family development regulations; however, it requests deviations from the standard unit count in a singular building and maximum height of the structure. The requested density is greater than the R-3 District standard but within the allowable range for Planned Housing Developments.

| | R-3 District Standard | PHD Standard* | Proposed by Applicant |
|--------------------------------------|-----------------------|-------------------|-----------------------|
| Maximum Height of Structures | 35 ft | | 43 ft |
| Maximum Units per Apartment Building | 12 units per building | | 14 units per building |
| Maximum Density | 10 units per acre | 20 units per acre | 13.79 units per acre |

*Planned Housing Development minimum standards per *Town Code Sec. 42-164*

Key Information

Zoning & Land Use: The project aligns with the Town's Comprehensive Plan and Future Land Use Map, located partially within the Downtown Urban Development Area, offering additional multifamily housing in an area deemed appropriate for higher density.

Height: The applicant is requesting a deviation in the maximum height of the main structures from 35 feet to 43 feet. Staff recommends an increase in minimum setbacks of one foot for each foot in height over 35 feet.

| Setback | R-3 District Standard Minimum | Increased Minimum Due to Height** | Actual Proposed by Applicant |
|------------------|-------------------------------|-----------------------------------|------------------------------|
| Front (from ROW) | 30 ft | 38 ft | 108 ft and 160 ft |
| Side | 10 ft | 18 ft | 33 ft |
| Rear | 20 ft | 28 ft | 24.9 ft*** |

** Potential increased minimum setback using *Town Code Sec. 42-160* as a model

*** Updated rear setback expected in revised narrative; not received as of this report

Potential Conditions

The applicant has not suggested any conditions. Town staff recommends the following conditions (* new since December 1 meeting):

1. The project shall be developed in general conformance with the masterplan submitted dated September 22, 2025.
2. The approved Conditional Use Permit shall only be for the specific residential use as described in this application.
3. Applicant shall maintain a row of no fewer than five evergreen screening trees along the shared boundary with Tax Parcel 526 – ((A)) – 136.
4. The required front, side and rear yards shall be increased one foot for each foot in height over 35 feet. *

Updates for Commission

Connection to Chrisman Street: Over the course of the site design process there have been two general layouts proposed. The first layout were townhomes, which included a connection on Chrisman. The second layout is significantly different with two apartment buildings and no connection. Per the attached email from Joshua Middleton, Assistant Director of Engineering, though the past and current layouts do not seem to support the connection, it is likely that a layout is feasible that would support a connection to Chrisman.

Closure of Angle Street: At the previous meeting, Commission asked staff to look into the feasibility of closing Angle Street. Staff has determined that the street closure would be detrimental to interconnectivity and emergency services access, and it would not provide adequate space for vehicles to turn around.

Street Width of First Street and Angle Street: Commission had asked staff to provide street widths for Angle Street and this portion of First Street. Staff measured and found the width of Angle Street ranged from 15 feet to 16 feet and the width of this portion of First Street ranged from 16 feet to 17 feet. If these streets were built today, based on current VDOT design standards, the minimum road width would be 18 feet with no on-street parking allowed. See attached VDOT Table “Geometric Design Standards for Residential and Mixed Use Subdivision Streets.”

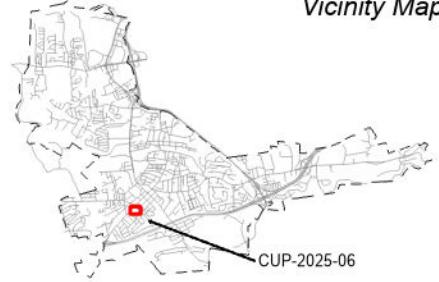
On-Street Parking: Staff examined the possibility of designating Angle and First Streets as “No Parking.” A site visit showed that existing residents regularly use the ample off-street right-of-way to park vehicles. There is sufficient room off the pavement and within right-of-way for vehicles to park without adversely impacting travel lanes. While the design of the streets supports signing as “No Parking,” staff does not support bringing this forward to Street Committee at this time.

Bus Service: Included with the packet is the Blacksburg Transit (BT) Christiansburg route map and schedule, retrieved from the Blacksburg Transit website 12/11/25. The map shows downtown service provided by both the Blue and Green (formerly Gold) routes.

Attachments Provided with the Staff Report:

- Aerial Map
- Zoning Map
- Future Land Use Map
- Blacksburg Transit Christiansburg Route Map
- Email from Joshua Middleton, Assistant Director of Engineering, dated 12/05/25
- VDOT Table “Geometric Design Standards for Residential and Mixed Use Subdivision Streets” from *VDOT Road Design Manual* - Appendix B Subdivision Street Design Guide

Vicinity Map



Aerial Map



SUBJECT
PROPERTY
526-((A))-138



Legend

■ CUP Request

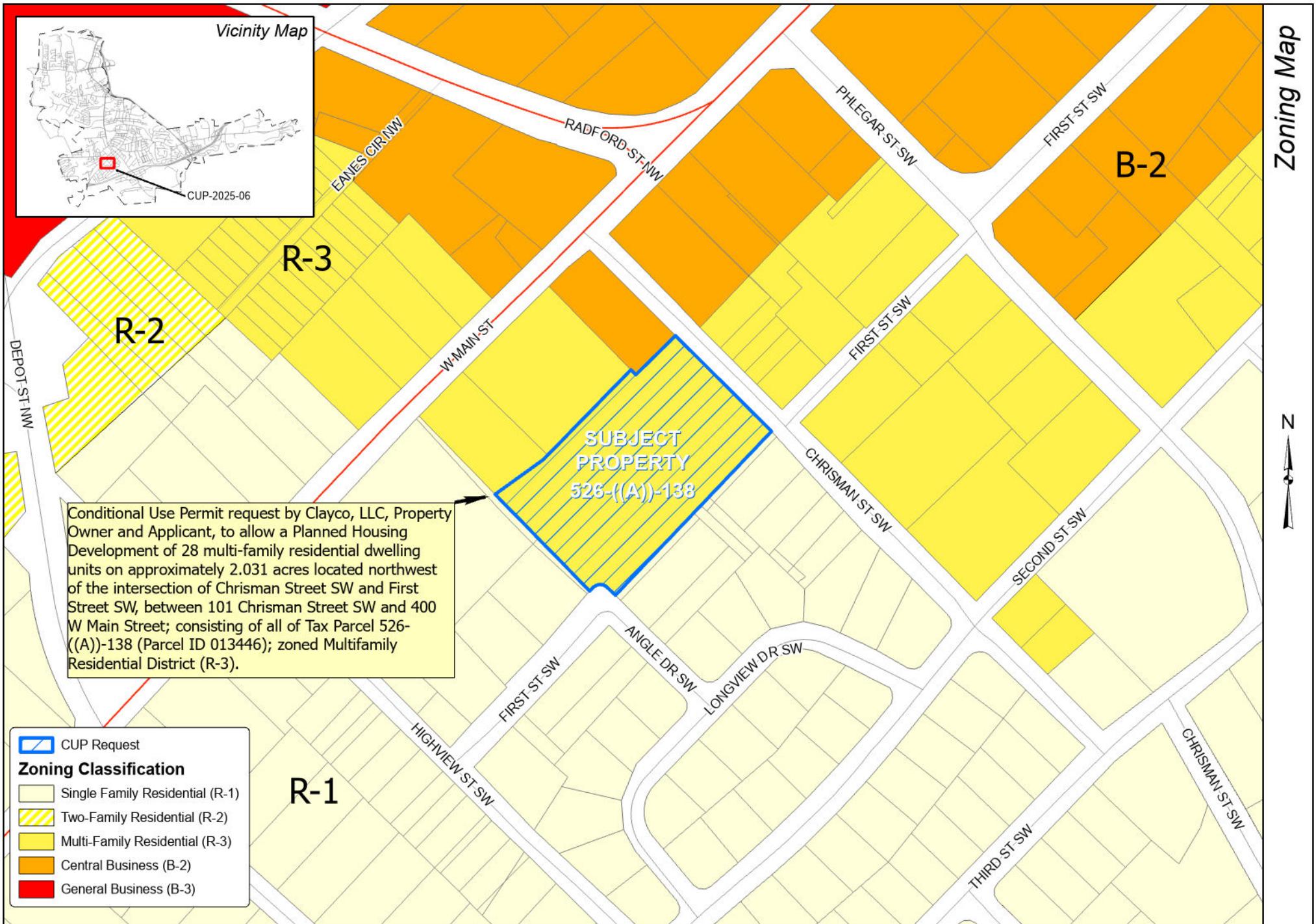
CUP REQUEST: CUP-2025-06 Chrisman St SW and First St SW

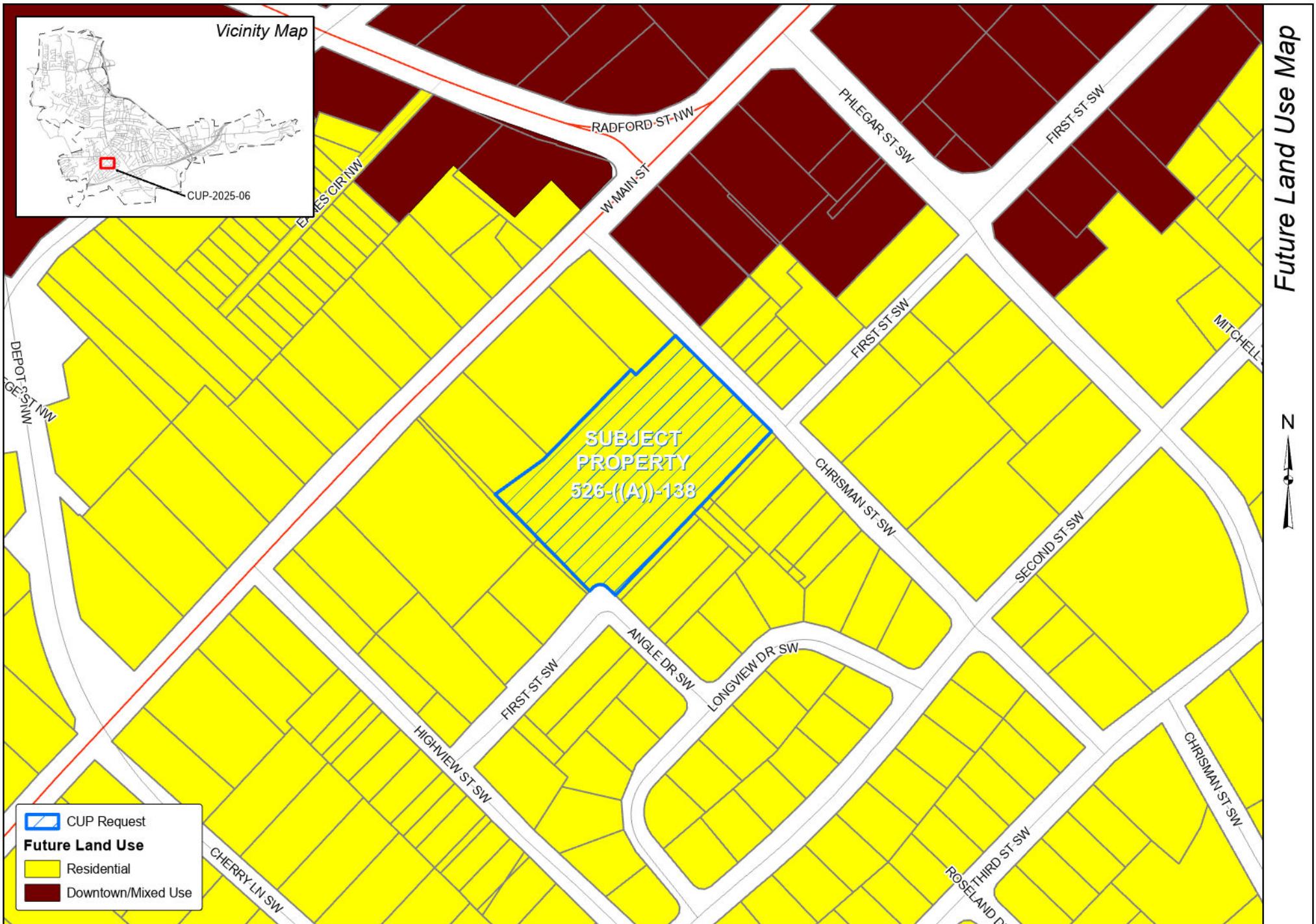
Planning Commission Public Hearing: December 1, 2025

Town Council Public Hearing: January 13, 2026

0 50 100 150 200 250 300
Feet







CUP REQUEST: CUP-2025-06 Chrisman St SW and First St SW

Planning Commission Public Hearing: December 1, 2025

Town Council Public Hearing: January 13, 2026

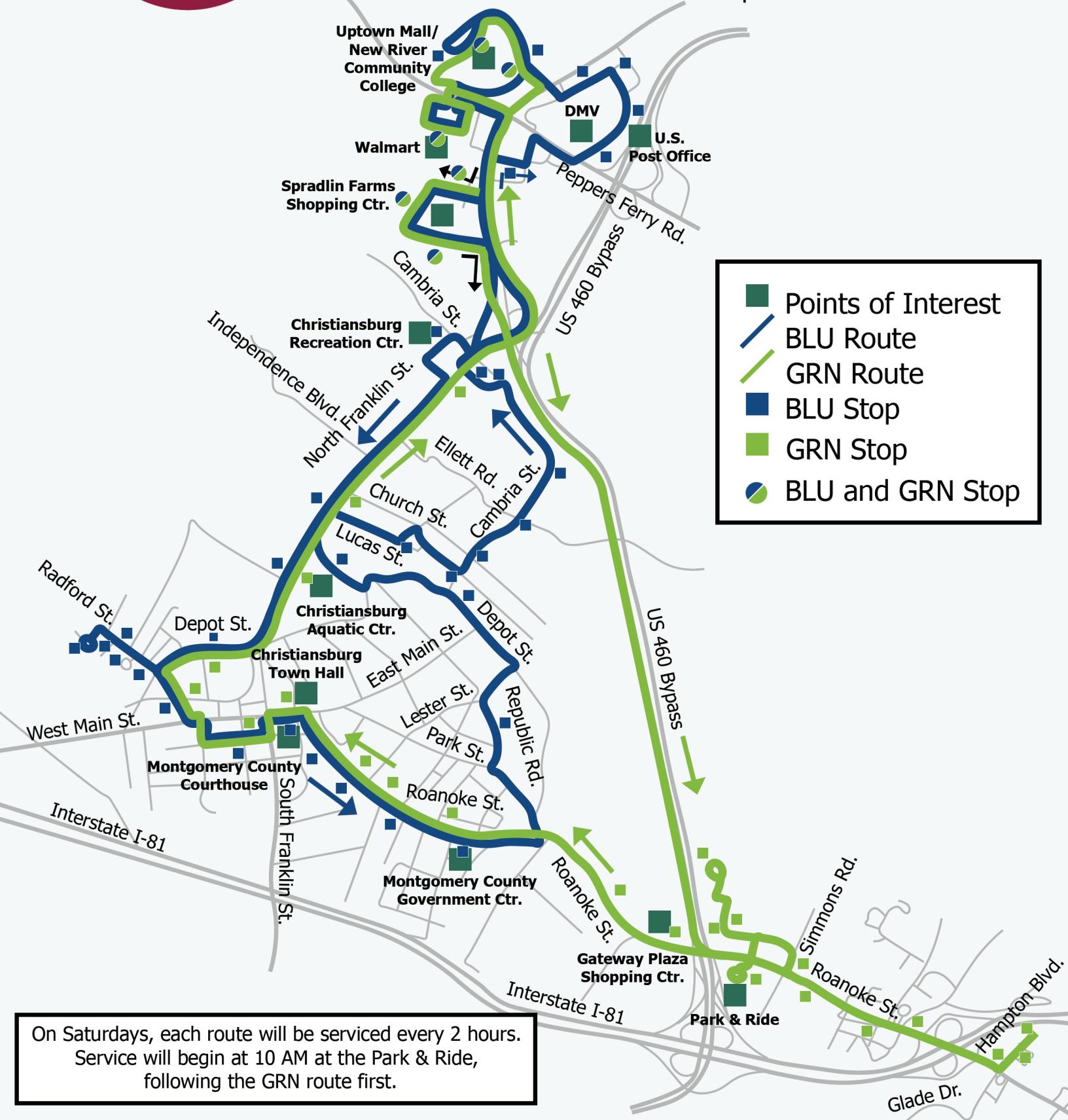




Christiansburg Map

BLU GRN

BLU serves Walmart **after** the Uptown Mall time check.
GRN serves Walmart **before** the Uptown Mall time check.



On Saturdays, each route will be serviced every 2 hours.
Service will begin at 10 AM at the Park & Ride,
following the GRN route first.



Explorer Blue Map

BLU

Provides service to the Uptown Mall area, N. Franklin St., Radford St., Downtown Christiansburg and Cambria area.



BLU

Provides service from Uptown Mall throughout Christiansburg.
BLU Service Area: North Franklin, Radford St., Downtown Christiansburg, Cambria area

**Changes: The GRN has replaced the GLD route. The GLD route is no longer in service.
Both routes, BLU & GRN, have additional timechecks. Buses do not leave the timecheck before the scheduled time.**

Weekdays

| Uptown Mall | Recreation Center | Montgomery County Government Center | Central / North Franklin |
|-------------|-------------------|-------------------------------------|--------------------------|
| Stop #1800 | Stop #2112 | Stop #2100 | Stop #2177 |

Route Frequency: Hourly

| | | | |
|-----------------------|-----------------|-----------------|-----------------|
| | | 7:05 AM | 7:20 AM |
| 7:45 AM | 7:55 AM | 8:10 AM | 8:20 AM |
| 8:40 AM | 8:50 AM | 9:05 AM | 9:20 AM |
| 9:40 AM | 9:50 AM | 10:05 AM | 10:20 AM |
| 10:40 AM | 10:50 AM | 11:05 AM | 11:20 AM |
| 11:40 AM | 11:50 AM | 12:05 PM | 12:20 PM |
| 12:40 PM | 12:50 PM | 1:05 PM | 1:20 PM |
| 1:40 PM | 1:55 PM | 2:05 PM | 2:20 PM |
| 2:40 PM | 2:55 PM | 3:05 PM | 3:20 PM |
| 3:40 PM | 3:55 PM | 4:05 PM | 4:20 PM |
| 4:40 PM | 4:55 PM | 5:05 PM | 5:20 PM |
| 5:40 PM | 5:55 PM | 6:05 PM | 6:20 PM |
| 6:40 PM Drop off only | | | |

Weekends

| Uptown Mall | Recreation Center | Montgomery County Government Center | Central North Franklin |
|-------------|-------------------|-------------------------------------|------------------------|
| Stop #1800 | Stop #2112 | Stop #2100 | Stop #2177 |

Route Frequency: 2 Hours

| | | | |
|-----------------|-----------------|----------------|----------------|
| 10:40 AM | 10:50 AM | 11:05 AM | 11:20 AM |
| 12:40 PM | 12:50 PM | 1:05 PM | 1:20 PM |
| 2:40 PM | 2:55 PM | 3:05 PM | 3:20 PM |
| 4:40 PM | 4:55 PM | 5:05 PM | 5:20 PM |

All stops listed are timechecks; buses do not leave these stops before the scheduled departure time.

Passengers should be at the bus stop at least 5 minutes before scheduled departure time.

Italics represent approximate times. Bold times indicate PM hours.

For Full, Intermediate or Reduced service information please visit the event calendar at www.ridebt.org



Explorer Green Map

GRN

**Provides service to the Uptown Mall area, N. Franklin St.,
Downtown Christiansburg and Roanoke St.**



GRN

Provides service from Uptown Mall throughout Christiansburg.
BLU Service Area: North Franklin, Radford St., Downtown Christiansburg, Cambria area

Changes: The GRN has replaced the GLD route. The GLD route is no longer in service.
Both routes, BLU & GRN, have additional timechecks. Buses do not leave the timecheck before the scheduled time.

Weekdays

| Uptown Mall | 118 Park & Ride | Town Hall | North Franklin / Central |
|--------------------------------|-----------------|----------------|--------------------------|
| Stop #1800 | Stop #2115 | Stop #2120 | Stop #2303 |
| Route Frequency: Hourly | | | |
| | 7:00 AM | 7:10 AM | 7:20 AM |
| 7:45 AM | 8:00 AM | 8:10 AM | 8:20 AM |
| 8:40 AM | 8:55 AM | 9:10 AM | 9:20 AM |
| 9:40 AM | 9:55 AM | 10:10 AM | 10:20 AM |
| 10:40 AM | 10:55 AM | 11:10 AM | 11:20 AM |
| 11:40 AM | 11:55 AM | 12:10 PM | 12:20 PM |
| 12:40 PM | 12:55 PM | 1:10 PM | 1:20 PM |
| 1:40 PM | 1:55 PM | 2:10 PM | 2:20 PM |
| 2:40 PM | 2:55 PM | 3:10 PM | 3:20 PM |
| 3:40 PM | 3:55 PM | 4:10 PM | 4:20 PM |
| 4:40 PM | 4:55 PM | 5:10 PM | 5:20 PM |
| 5:40 PM | 5:55 PM | 6:10 PM | 6:20 PM |
| 6:40 PM Drop off only | | | |

Weekends

| Uptown Mall | 118 Park & Ride | Town Hall | North Franklin / Central |
|---------------------------------|-----------------|-----------------|--------------------------|
| Stop #1800 | Stop #2115 | Stop #2120 | Stop #2303 |
| Route Frequency: 2 Hours | | | |
| | 10:00 AM | 10:10 AM | 10:20 AM |
| 11:40 AM | 11:55 AM | 12:10 PM | 12:20 PM |
| 1:40 PM | 1:55 PM | 2:10 PM | 2:20 PM |
| 3:40 PM | 3:55 PM | 4:10 PM | 4:20 PM |
| 5:40 PM | 5:55 PM | 6:10 PM | 6:20 PM |
| 6:40 PM Drop off only | | | |

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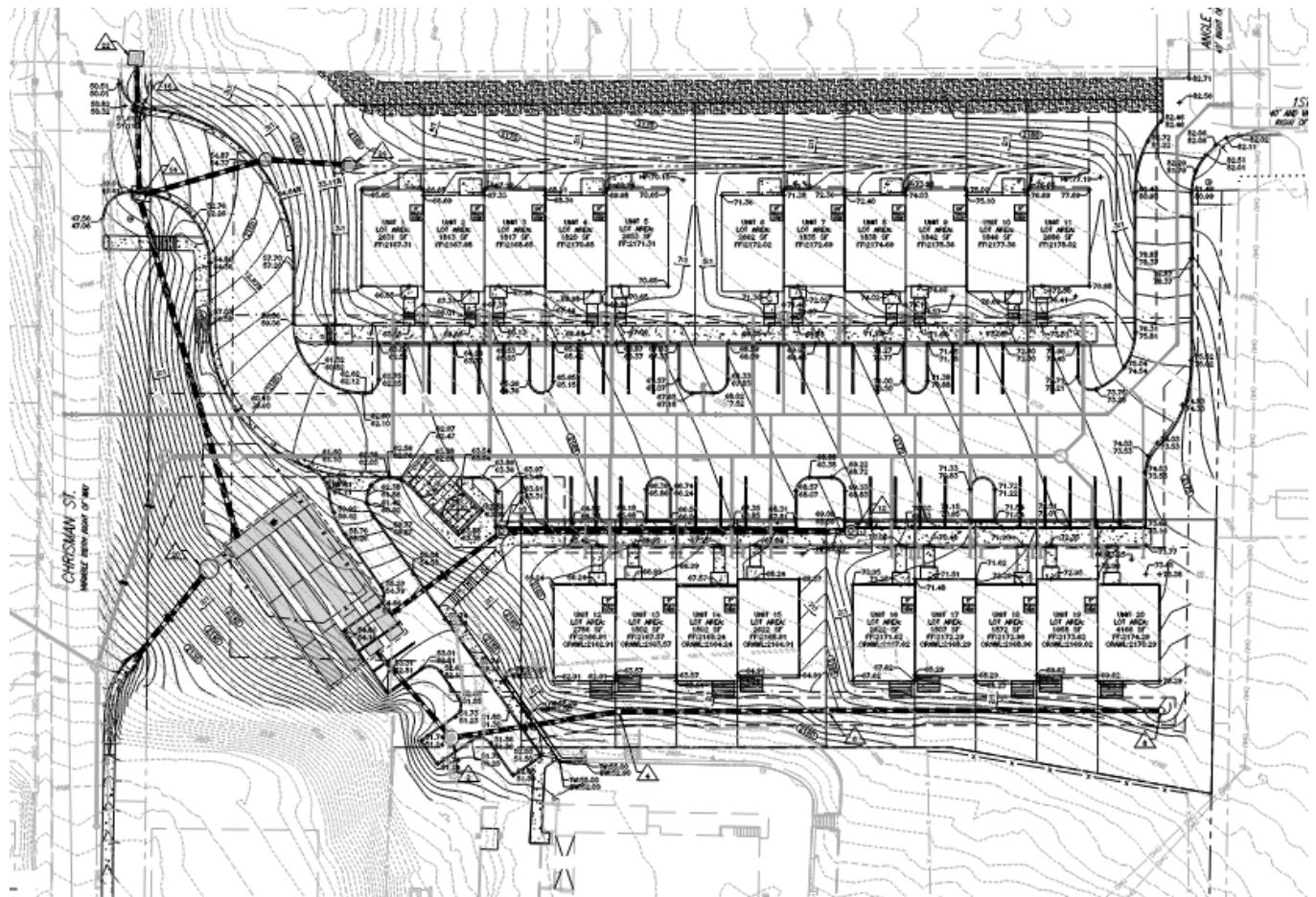
From: Joshua P. Middleton
Sent: Friday, December 5, 2025 3:10 PM
To: Devon Shields
Cc: Retta Jackson; Mike Kelley
Subject: Re: Chrisman & West Main Street Development

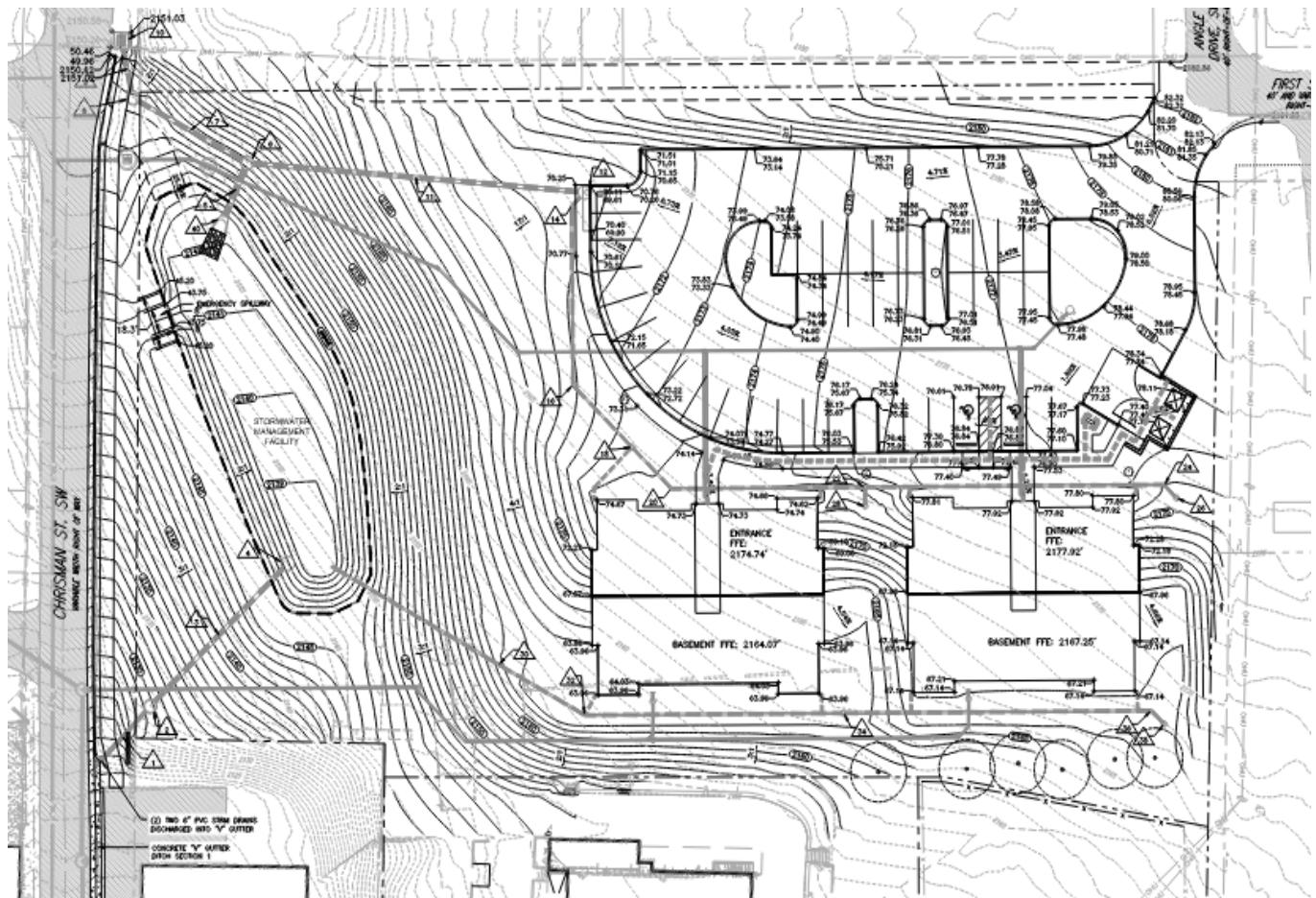
Hey Devon,

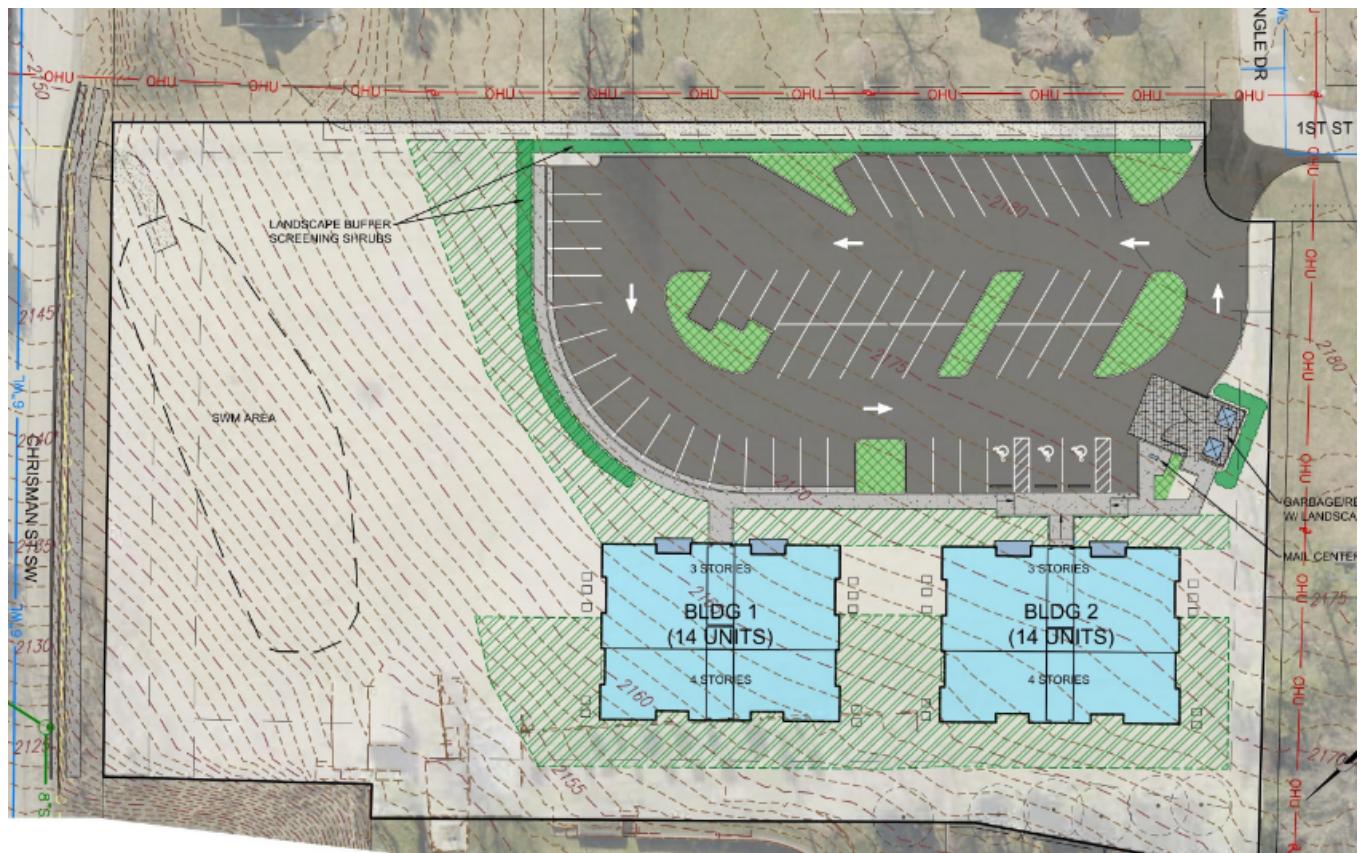
I apologize that I am coming into this on the back end and do not have all of the context or previous conversation; however, I can offer a couple of things. It appears that over the course of the site design process there have been two general layouts proposed. The first layout were townhomes, which included a connection on Chrisman. The second layout is significantly different with two apartments and no connection. It appears the first layout connection did become infeasible due to the proposed layout and design requirements for the connection.

When the layout was revised to apartments, the SWM facility they utilized made a connection infeasible due to its size, and orientation along Chrisman. The original layout included an underground facility where as the revised layout utilizes above ground detention through the use of a 'dry pond'. In order for a connection to be provided, the proposed SWM configuration would need to be reconsidered and redesigned at which time a connection to Chrisman would likely become feasible.

So to summarize, the ability to provide a connection is highly dependant upon the proposed layout, particularly the SWM facility design. Though the past and current layouts do not seem to support the connection it is likely that a layout is feasible that would support a connection to Chrisman.







Hope this helps,

Joshua Middleton

Assistant Director - Engineering

Town of Christiansburg | 100 East Main Street | Christiansburg, VA 24073

Office: (540) 382-6120 ext. 1166



GEOMETRIC DESIGN STANDARDS FOR RESIDENTIAL AND MIXED USE SUBDIVISION STREETS (GS- SSAR)

| PROJECTED TRAFFIC VOLUME (ADT) | MINIMUM DESIGN SPEED (MPH) (NOT POSTED SPEED) | HORIZONTAL AND VERTICAL CONTROLS | | | | SHOULDER AND DITCH ROADWAYS | | | | |
|--------------------------------|--|----------------------------------|-------------------------------|-----------------|------------------------|-----------------------------|------------------------|--------------------|------------------------|----|
| | | Maximum 2:1 Cut or Fill Slope | | MAXIMUM % GRADE | MINIMUM SIGHT DISTANCE | | MINIMUM PAVEMENT WIDTH | | | |
| | | CURVE DATA | MINIMUM CENTERLINE RADIUS (4) | | STOPPING (2) | INTER-SECTION (3) | NO PARKING (5) | PARKING 1 SIDE (5) | PARKING BOTH SIDES (5) | |
| UP TO 2000 | 25 | 200' | NONE | NOTE (6) | 155' | 280' | 24' (1) (11) | 24' (1) | 29' (1) | 3' |
| 2001 TO 4000 | 30 | 335' | NONE | NOTE (7) | 200' | 335' | 26' (10) | 31' (10) | 36' (10) | 6' |

Notes:

For streets with volumes over 4000 or serving heavy commercial or Industrial traffic; use the appropriate geometric design standard. (see [VDOT's Road Design Manual](#))

The roadway with the highest volume will govern the sight distance.

Right of Way requirements can be found in [Section 4.M Right Of Way](#)

For volumes 2001 – 4000 vpd, design criteria for the Collector functional class was utilized to determine minimum design values.

Lower design speeds (and street widths) may be utilized provided they are designed in accordance with the AASHTO Green Book or AASHTO's Guidelines for Geometric Design of Low-Volume Roads*. The designer should coordinate with VDOT in advance of design (e.g. sketch plan stage) if this alternative criteria is being utilized.

If 20 mph minimum design speed is utilized, a 20 mph advisory speed limit sign shall be posted along with any other horizontal or vertical curve warning signs as warranted.

An engineering speed study sealed and signed by a licensed professional engineer, using VDOT's standard speed study report, must be provided by the developer and approved by VDOT for any roads posted at other than the statutory speed limit and planned for acceptance into the state system.

1. If the Local Street has 1 point of access and ADT>400 vpd, then the roadway width must meet design values (2001 TO 4000 vpd).
2. 2018 AASHTO Green Book Chapter 3 (Table 3-1)
3. 2018 AASHTO Green Book Chapter 9 (Table 9-7). For grades greater than 3%, the time gap must be adjusted and required sight distance recalculated.
4. 2018 AASHTO Green Book Chapter 3 (Table 3-13)
5. Clear zone width for UP TO 2000 vpd is 7' and clear zone values for 2001 TO 4000 vpd is 10'.
6. 2018 AASHTO Green Book Chapter 5 (Section 5.3).
7. 2018 AASHTO Green Book Chapter 6 (Section 6.3).
8. 2018 AASHTO Green Book Chapter 5 (Table 5-5)
9. Add an additional '4' if guardrail is required.
10. Lane widths may vary between 10'-12' feet for collectors with 2001-4000 ADT. Widths shown may be decreased by 2 feet (26 feet to 24 feet), (31 feet to 29 feet) and (36 feet to 34 feet) based upon engineering judgment subject to VDOT approval.
11. For 0-400 ADT and "No Parking" ONLY minimum pavement width may be reduced from 24 feet to 18 feet and shoulder width may be reduced in accordance with Note 8 above.



THE PLACE TO BE.
CHRISTIANSBURG VA
DEPARTMENT OF PLANNING

100 East Main Street
Christiansburg, VA 24073
p: (540) 382-6120
f: (540) 381-7238

Conditional Use Permit Application

Landowner: CLAYCO LLC

Address: P O BOX 10397
BLACKSBURG VA 24062

Phone: 540-953-2080

Applicant: CLAYCO LLC

Address: P O BOX 10397
BLACKSBURG VA 24062

Phone: 540-953-2080

I am requesting a Conditional Use Permit to allow a Planned Housing Development for a multi-family residential project in a R-3 zoned district. on my

property that is zoning classification R-3 under Chapter 42: Zoning of the Christiansburg Town Code.

My property is located at Chrisman Street

Tax Parcel(s): 526- A138

Parcel Number(s): 013446

Fee: \$750.00

I certify that the information supplied on this application and any attachments is accurate and true to the best of my knowledge. I understand that Conditions may be placed on my property in regards to the above mentioned use/activity. I also understand that the Conditional Use Permit may be revoked and/or additional Conditional Use Permits required should questions regarding conformity arise.

Signature of Landowner(s):

Date: 9/22/2025

Date: _____

Date: 9/22/2025

Date: _____

This request was approved/disapproved by a vote of the Christiansburg Town Council on _____.

Any Conditions attached shall be considered requirements of the above request.

Town Manager

Date



Please complete the following section:

Per Section 42-8(e) and 42-8(f) of the Christiansburg Town Code:

Conditional use permit application submissions shall include a traffic impact statement whenever a proposed conditional use permit substantially affects transportation on town streets through traffic generation of either:

- (1) *100 vehicles trips per peak hour by residential development;*
- (2) *250 vehicles trips per peak hour by non-residential development; or*
- (3) *2,500 vehicle trips per day by non-residential development.*

The data and analysis contained in the traffic impact statement shall comply with Virginia Department of Transportation Traffic Impact Analysis Regulations 24 VAC 30-155-60 and all applicable town ordinances.

A traffic impact analysis is is not required for the proposed project:

1. Yes or No, the proposed residential development generates 11 vph which is greater than the requirement of 100 vehicles per hour, or
2. Yes or No, the proposed non-residential project generates _____ vph which is greater than the requirement of 250 vehicles per hour
3. Yes or No, the proposed non-residential project generates _____ vpd which is greater than the requirement of 2,500 vehicles per day.
4. Yes or No, a new TIS study is not required because a previously submitted TIS is still applicable for the project site. (Note: the appropriate documentation must be attached)

***Trip generation shall be measured based on the current edition of the Institute for Transportation Engineers (ITE) Trip Generation Manual**

CONDITIONAL USE PERMIT APPLICATION FOR

Chrismen & West Main Street
Planned Housing Development

DECEMBER 12, 2025

PREPARED FOR:



PREPARED BY:





Project Narrative

Chrisman & West Main Street

Planned Housing Development

Christiansburg, Virginia

Prepared For:

SAS Builders, LLC
PO Box 10397
Blacksburg, VA 24062

Prepared By:

Balzer & Associates
Westwood Professional Services
80 College Street, Ste H
Christiansburg, VA 24073
(540) 381-4290

Project Number: 24220107.00

Date: December 12, 2025

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1.0 Executive Summary

1.1 Existing Zoning & Land Use

The site is currently zoned as Multifamily Residential (R-3) and is currently being used as a vacant site. Two properties adjacent to the north of this project are also zoned R-3. One is being used as a single family home and one is a multi-family residential building. One neighboring property on the corner of West Main Street and Chrisman Street (400 West Main, Parcel 004688), which is zoned Downtown Commercial (B-2) and is being used as the Montgomery County Emergency Assistance Program (MCEAP) office and food bank. Adajecnt properties to the south-east and south-west are zoned Single Family Residential (R-1) and are being used as such. Across Chrisman Street is a row of five townhomes, zoned Multifamily Residential (R-3).



Figure 1.1 – Existing Zoning Map

1.2 Proposed Development

This application is for a conditional use permit to allow a Planned Housing Development for a multi-family use within an existing R-3 district. The project is proposed as two multi-family buildings with a total of 14 units per building. Each building will have a mix of one, two and three bedroom units. The buildings will be designed as a three/four split story building.

1.3 Proposal Overview

| Item | Summary |
|----------------------|---|
| Site Acreage | 2.03 |
| Total Units Proposed | 28 (8 - 1 bedroom, 14 - 2 bedroom, 6 - 2 bedroom) |
| Phasing | None |

1.4 Development Regulations

| Item | Town Requirements | Proposed Development |
|-------------------|-----------------------------|--|
| Density | 20 units per acre maximum | 13.79 units per acre |
| Front Setback | 30ft from all Right-of-Ways | ±160ft from Chrisman St, ±108ft from Angle Drive/First Street |
| Side Setback | 10ft | 10ft – (Actual 27ft) |
| Rear Setback | 20ft | 20ft – (Actual 33 ft) |
| Parking Setback | 15ft from Right-of-Way | 15ft – (Actual 36.5ft) |
| Maximum Height | 35ft | 43ft |
| Required Frontage | 125 linear feet | 217.7ft |

1.5 Land Use

| Item | Summary |
|------------------------------|---|
| Existing Land Use | Vacant |
| Future Land Use (Comp. Plan) | Residential |
| Existing Zoning | R-3 |
| Proposed Zoning | R-3 with a Conditional Use Permit for Planned Housing |

1.6 Transportation & Parking

| Item | Summary |
|--|---|
| Parking Required | 56 spaces |
| Parking Proposed | 56 spaces |
| Parking Rate Required | 2.0 spaces per unit |
| Parking Rate Provided | 2.0 spaces per unit |
| Accessibility Parking Required | 2 spaces |
| Accessibility Parking Proposed | 3 spaces |
| Nearest Transit Stop(s) | Radford & College Street |
| Walking Distance to Nearest Transit Stop | Appx. 0.15 - 0.28 miles walking distance to site. |
| Pedestrian Infrastructure | A new sidewalk is proposed along Chrisman Street. |
| VDOT TIA Required? | No |
| Town TIA Required? | No |
| Traffic Impacts | None |

1.7 Landscaping & Open Space

| Item | Summary |
|---------------------------------|---|
| Parking Lot Greenspace Required | 2,240 square feet (40 sf / parking space) |
| Parking Lot Greenspace Proposed | 2,300 square feet |
| Open Space Required | 11,200 square feet (400 sf / dwelling unit) |
| Open Space Proposed | 12,250 square feet |

2.0 Preliminary Layout

2.1 Zoning

This application is for a conditional use permit to allow a Planned Housing Development for a multi-family use within an existing R-3 district. The project is proposed as two multi-family buildings with a total of 14 units per building. Each building will have a mix of one, two and three bedroom units. The buildings will be designed as a three/four split story building.

2.2 Master Plan

Appendix Sheet Z3 shows the Master Plan of the proposed development. The Master Plan graphically designates the location of proposed access roads, building lots, open space, public utilities, and stormwater management areas. Specific design elements of the project are discussed in more detail in the following portions of this application. The project's direct correlation to guiding principles of the Town of Christiansburg Comprehensive Plan is discussed in Section VI entitled Zoning, Existing Land Use, and Comprehensive Plan Vision.

3.0 Site Development Regulations

As the project is proposed to be a multi-family development within the existing R-3 zoning district and is requesting a Conditional Use Permit for Planned Housing, the development will be designed per the standards listed below. The standards listed below will keep with the by-right standards for R-3 unless noted otherwise.

3.1 Setbacks

The proposed development does not require any changes to the underlying setbacks of the R-3 zone.

| Building Setbacks | R-3 Typical | Proposed Changes in Development |
|---|---|---------------------------------|
| Front (from ROW) | 30ft from all Right-of-Ways | No changes proposed. |
| Side | 10ft | No changes proposed. |
| Rear | 20ft | No changes proposed. |
| Parking Lots from ROW | 15ft | No changes proposed. |
| Accessory Structure Side | 3ft | No changes proposed. |
| Accessory Structure Rear | 3ft | No changes proposed. |
| From driveways, aisles, & parking areas | 15ft; except for windowless walls, for which parking areas may be 5ft and driveways/aisles may be 10ft. | No changes proposed. |
| From other buildings on same lot | 25ft | No changes proposed. |

3.2 Heights

| Maximum Heights | R-3 Typical | Proposed Changes in Development |
|----------------------|---|---------------------------------|
| Primary Structures | 35ft, except as described in 42-160.* | 43ft |
| Accessory Structures | 1 story, except as described in 42-160 Error! Bookmark not defined. | No changes proposed. |

3.3 Density

3.3.1 Maximum Residential Unit Density

The proposed development will be slightly over the typical R-3 allowed density. The PHD Conditional Use Permit is proposing a density of 13.79 units per acre.

| R-3 Typical | PHD Typical | Proposed Development |
|--------------------|-------------------|----------------------|
| 10 units per acres | 20 units per acre | 13.79 units per acre |

3.3.2 Maximum Units Per Building

The proposed development will have slightly more units than the typical R-3 zoning of 12 units per building.

| R-3 Typical | Proposed Development |
|-------------|----------------------|
| 12 | 14 |

3.3.3 Maximum Occupancy

No changes are proposed to the typical R-3 zoning regarding maximum occupancy. In other words, the proposed R-3 District and Planned Housing Development shall have a maximum dwelling unit occupancy requirement of a family plus two (2) unrelated individuals per unit.

3.4 Project Phasing

This project is not proposed to be phased.

* "(2) Church spires, belfries, cupolas, monuments, water towers, chimneys, flues and flagpoles shall be exempt from this section."

(3) Parapet walls may be up to four feet above the height of the building on which the walls rest...

(4) Accessory buildings shall not exceed the main structure in height, except that when the accessory building is located at a lower ground elevation, then the elevation of the roofline of the accessory building shall not be higher than the elevation of the roofline of the main structure, but not to exceed two stories in height." – Code of the Town of Christiansburg, Virginia; Sec. 42-160

3.5 Subdividing & Parcels

No subdivisions are proposed for this project.

3.6 Landscaping & Screening

3.6.1 Preservation of Existing Trees

Six trees will be preserved along the boundary with parcel 013466 (408 W Main St) should grading allow. Five of these are evergreens. These are proposed to function as a screen between the new development and the existing single family homes along Main Street.

3.6.2 Parking Lot Landscaping

2,300 square feet of parking lot landscaping is proposed and shall include trees or shrubs, per Town of Christiansburg requirements. These are provided through landscape islands as shown on Sheet Z3.

3.6.3 Open Space

According to the Town of Christiansburg Zoning Ordinance for apartment uses, “At least 400 square feet of commonly usable open space shall be provided for each dwelling unit. Such space shall be of such location and dimensions as to provide for outdoor living, patios, pools, lawns, play areas, walks, wooded areas, and the like, but not including driveways and parking areas.” There is a total of 24 apartments units proposed for this project. Based on the required ratio, a minimum of 11,200 s.f. (0.6 acres) shall be designed as open space. The attached exhibit Sheet Z3 shows the areas designated as open space. The site will contain more than 12,350 square feet of property that could qualify as open space.

3.7 Site Lighting

The parking lot will be lit by four small parking lot lights. Per town requirements, they will be arranged and installed to minimize glare on the adjacent properties. It will not exceed the height maximum of 15ft in a residential district. All lighting will be provided as required by Town of Christiansburg zoning ordinance and in coordination with AEP.

3.8 Maintenance

The applicant will be responsible for maintenance of all common space elements including exterior elements including open space and stormwater management areas. These areas will be under the development’s ownership or an affiliate and will be maintained at no cost to the general taxpayer.

3.9 Signage

The developer reserves the right to construct a project identification sign at a location to be determined during the final construction plan development and approval process. Any proposed signage will be permitted separately, and the designs and sizes will meet the signage requirements as stated within the Town of Christiansburg zoning ordinance.

3.10 Miscellaneous Provisions

- Drive aisles and parking lots will be designed and constructed in accordance with the Town of Christiansburg Zoning Ordinance
- Sidewalks will be provided along Chrisman Street as required by Town Code.

4.0 Public Utilities

All utilities will be constructed to Town standards, and where appropriate, be dedicated to the Town. Public utility easements will be dedicated along water distribution and sewer collection lines outside of the road right-of-way.

4.1 Water

4.1.1 Domestic Service

The development is proposed to connect to the existing six-inch water line in Chrisman Street, near the easternmost corner of the property with a six-inch tapping sleeve and valve. Per typical development standards, a water meter and a backflow preventer will be installed near this same corner of the property. The line will travel nearly due west towards the parking lot, at which point, the line will bend to travel underneath the parking lot. From the parking lot, a two-inch domestic service water lateral line will connect into each of the two buildings.

4.1.2 Fire Prevention Service

A fire hydrant will be located at the end of the six-inch water line discussed above. This hydrant will be in the south-westernmost landscaping island in the parking lot. It will be approximately 141 linear feet and 65 linear feet to the fire department connections on each building.

4.2 Sewer

4.2.1 Overall Description

The development is proposed to connect to the existing eight-inch sewer line on Chrisman Street, near the north-easternmost corner of the property. A new six-inch lateral is proposed to connect into the existing manhole. The line will run roughly parallel to the property line to the buildings.

4.2.2 Estimated Usage

Based on local and state Standards, an average daily flow is estimated for the proposed uses below.

| Land Use | Expected Usage Rate | Multiplier | Estimated Usage |
|-----------------------|------------------------|-------------------------|-----------------|
| Apartments (20 units) | 100 gal/day per person | 48 persons (2 per unit) | 4,800 gal/day |

TOTAL ESTIMATED WATER/SEWER USAGE BY PROPOSED DEVELOPMENT = 4,800 gallons per day

Applicant will construct or cause to be constructed at no expense to the Town all water/sewer mains and appurtenances on the Property and will connect the water/sewer mains to publicly owned water/sewer mains. All water mains and sewer mains will be constructed to the standards of the Town, will comply with the regulations and standards of the Town, and will comply with the regulations and standards of all other applicable regulatory authorities. All water mains and appurtenances and sewer mains will be dedicated to public use unless otherwise directed by the Town of Christiansburg. Any water mains and appurtenances and/or sewer mains that must be relocated as part of the development will be relocated by the applicant at no cost to the Town.

4.3 Water Quality & Stormwater Management Standards

4.3.1 Existing Run-Off

The subject property shown for development naturally has two runoff patterns. The majority of the project site drains to the east towards Chrisman Street. This drainage flows north towards West Main Street where it is captured by curb inlets on either side of the street. This drainage area is within the Crab Creek Rivershed, which is part of the Upper New River watershed area.

As development occurs and impervious areas increase on the project site, stormwater management will be required to control the increased water flows as they move offsite towards Chrisman Street and downstream areas. The proposed stormwater management facility would be sized to accommodate the additional stormwater runoff created by the increased impervious areas of the development and designed to reduce the amount of post development runoff. It is anticipated that water quality requirements for the project will be achieved through a combination of onsite measures and the purchase of nutrient credits. If, during final design, the project is found to be within the threshold limits allowing for nutrient credit purchase, it is anticipated that the requirement will be handled with credits. The proposed stormwater management areas will conform to all applicable Department of Environmental Quality regulations dealing with stormwater quantity and quality. At a minimum, the 2-Year and 10-Year post-development runoff rates will be less than or equal to the 2-Year and 10-Year pre-development runoff rates, and all current channel and flood protection requirements set by the Virginia Stormwater Management Program from the Department of Environmental Quality and the Town of Christiansburg will be met. Downstream adequacy will also be addressed with the overall stormwater management plan to ensure areas downstream of the project site do not see increased flooding or erosion as noted earlier in this section.

4.3.2 Infrastructure Improvements

A new stormwater management facility will be constructed in the form of a detention pond designed to hold the water from the 10-year storm event. Surface water from the roofs of the buildings and the parking lot will drain into this pond via two pipes. Additionally, water draining along Chrisman Street, uphill from this development (south) will be piped into this pond. Curb and gutter will be installed along Chrisman Street, to facilitate the drainage of excess water from the pond.

4.4 Environmental Impacts & Concerns

There are no known specific areas of environmental impact or concern on the property. However, prior to site plan development, the property will be investigated to confirm there are no streams or wetlands on the site. If any are found, they will be confirmed by the US Army Corps of Engineers and the Department of Environmental Quality, and all appropriate permits filed, and mitigation provided, as necessary. During construction of the stormwater management area and perimeter grading, it will

also be necessary to provide all required erosion and sediment control measures to avoid any sediment and silt from reaching adjacent properties and College Street.

4.5 Trash Services

This project is proposing a dumpster facility to be enclosed and screened per Town requirements. This facility will be in the parking lot near the western building. It will be serviced by a private collection company. No individual trash cans or recycling cans are proposed.

4.6 Other Utilities

Utility connections such as power, phone, cable television, gas, and any other miscellaneous utilities serving this community shall be located underground. Relocation of any existing utility lines will be at the direction of the individual utility companies. Coordination with AEP and the other private utility companies will be necessary to limit or avoid impacts to the adjacent residential areas during construction

5.0 Traffic Circulation

5.1 Public Roads, Access Drives and Vehicular Traffic

The proposed site will be accessed by a drive aisle connecting to the bend at the intersection of First Street and Angle Drive. No access will be possible by Chrisman Street. All proposed internal drive aisles will be designed to meet Town standards.

By completing the Town of Christiansburg VDOT Traffic Impact Analysis (TIA) Supplemental Application, it has been determined that a TIA is not warranted with this project. Based on the limited number of proposed lots and the multiple ways to access the property, no negative traffic impacts to existing infrastructure are projected and no traffic improvements are proposed. The trip generation numbers for the proposed uses are shown below for the AM Peak, PM Peak and Weekday totals based on the average rate as listed in the 2011 ITE Manual.

Table 5.1.A - Traffic Impact Analysis

| Use | ITE | # Units | AM In | AM Out | AM Total | PM In | PM Out | PM Total | Weekday |
|--------------------------------|-----|---------|-------|--------|----------|-------|--------|----------|---------|
| Multifamily Housing (Mid Rise) | 221 | 28 | 2 | 8 | 10 | 7 | 4 | 11 | 127 |

5.2 Pedestrian Walkways

A five-foot wide public sidewalk will be installed along Chrisman Street on the west side per Town standards. Private sidewalks are proposed internal to the site to facilitate pedestrian traffic from the parking lot to the buildings.

6.0 Comprehensive Plan

The elements of this project that directly conform to the issues and principles stated in the Christiansburg Comprehensive Plan are listed below with reference to the policy chapter as updated on November 19, 2013. The italicized text is quoting from the Comprehensive Plan, while the regular text discusses how this proposal meets these guidelines.

6.1 Comprehensive Plan Maps

6.1.1 Future Land Use Map

The Future Land Use Map has designated this parcel as ‘Residential.’ This designation is described in the 2013 Comprehensive Plan,

“Residential development is the predominant land use in these areas. Type and intensity of housing development will be determined by zoning classification and other constraints.”

– Christiansburg Comprehensive Plan, Page IX-9

Generally, development surrounding the property is also designated as ‘Residential.’ To the north of this property, surrounding Main Street, is designated as ‘Downtown/Mixed Use,’ which is described in the Comprehensive Plan,

“Mixed use areas allow for residential and non-residential development in one area. The ratio of residential to non-residential uses in these areas varies. Mixed use may indicate vertically mixed uses within a building or horizontally mixed uses within a larger development area. Mixed use denotes pedestrian scale and orientation, complementary uses, and promotion of a 24 hour footprint.”

– Christiansburg Comprehensive Plan, Page IX-9

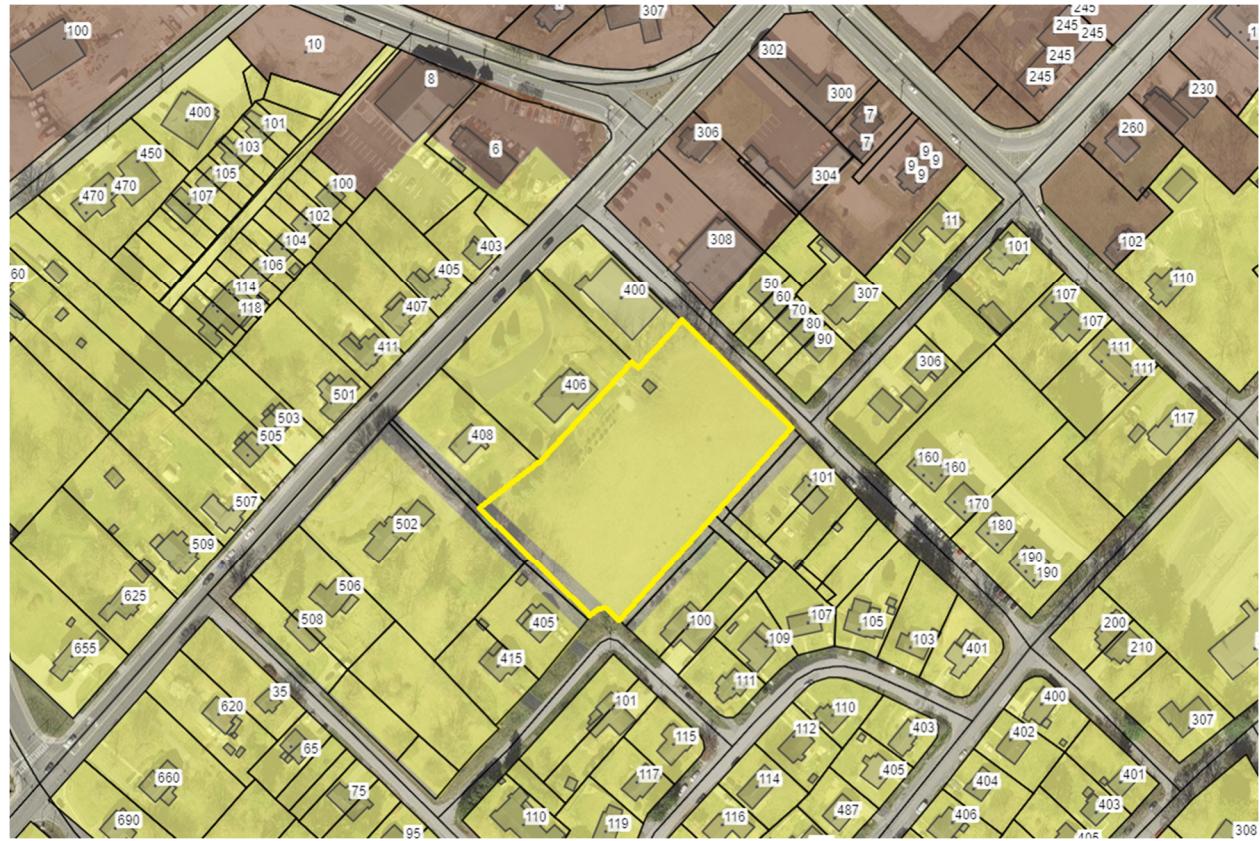
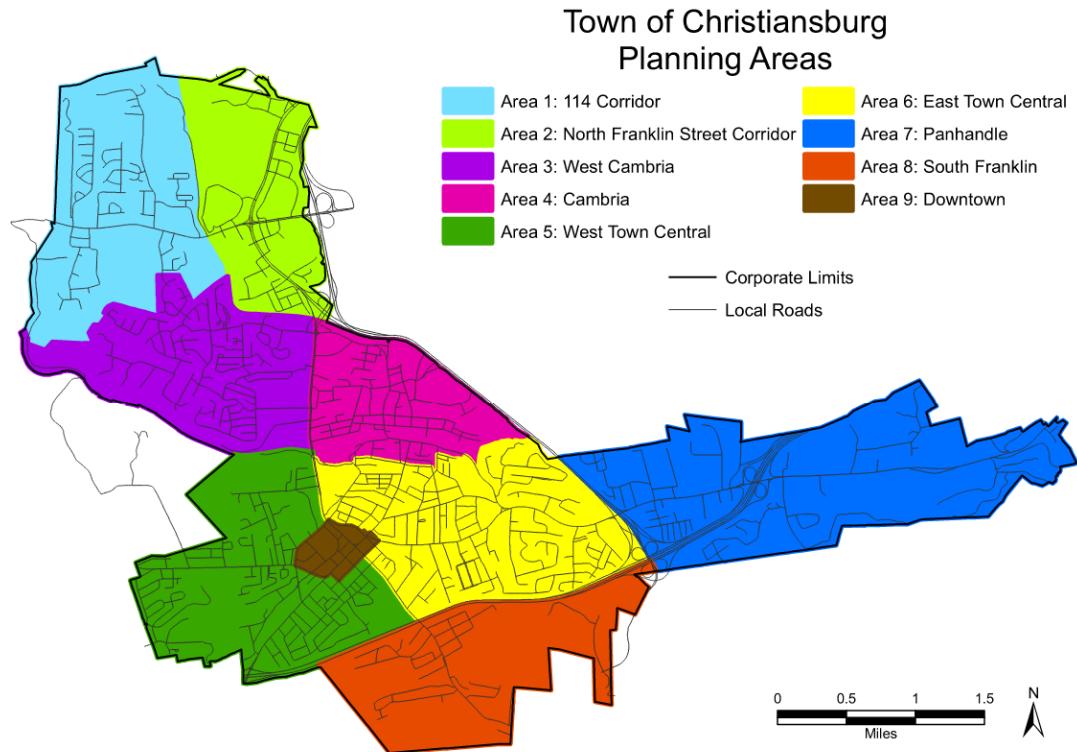


Figure 6.1.1 – Future Land Use Map

6.1.2 Planning Areas Map

The property is currently zoned R-3. The property is located in an area designated Area 5: West Town Central as shown on the Town of Christiansburg Planning Area Map in the Comprehensive Plan.



Map prepared by Town of Christiansburg Engineering and Planning Departments. November 8, 2013.
T:\GIS\Planning\CompPlanMaps\2013Update\LandUse\PlanningAreas.mxd

Figure 6.1.2 – Town of Christiansburg Planning Areas

6.2 Comprehensive Plan Policies

The elements that directly conform to the issues and principles stated in the Christiansburg Comprehensive Plan are listed below and reference the Policy Chapter as updated November 19, 2013.

6.2.1 Natural Resource Goals & Strategies

| Number | Goal/Strategy | Expected Outcome of Project |
|---------|--|--|
| ENV 1.2 | Encourage the retention of existing trees and wooded lots and the planting of additional trees during development. | Six trees will be preserved on the site, five of which are evergreen trees. These are intended to remain as a functional screen. |
| ENV 1.5 | Require recognition of critical features in development plans and locations for future development. | Critical features will be identified during the design development process. |

| | | |
|---------|--|---|
| ENV 1.8 | Limit development on sites with steep slopes and prohibit development that creates steep slopes. | Steep slope development is not anticipated with this proposal. |
| ENV 1.9 | Ensure soil type identification for all new developments and verify the use of safe development practices. | Soil types will be identified and verified for appropriate development as proposed. |

6.2.2 Water Quality, Watersheds, & Stormwater Management Goals & Strategies

| Number | Goal/Strategy | Expected Outcome the of Project |
|-----------|---|--|
| ENV 2.10 | Maintain standards and specifications for design and construction of stormwater management infrastructure. | Stormwater management will be implemented with the development that meets or exceeds state and local regulations and specifications. |
| ENV 2.13 | Reduce stormwater runoff and prevent flooding at existing sites by requiring upgrades with redevelopment or rezoning. | Stormwater management will be implemented with the development that meets or exceeds state and local regulations. |
| ENV 2.17B | Encourage the retention of existing trees and wooded lots and the planting of additional trees during development. | Six trees will be preserved on the site. Four new trees will be planted. |
| ENV 2.18 | Minimize private sewage facilities to the extent practical. | The project will be connected into the public sewer system. |

6.2.3 Open Space Goals & Strategies

| Number | Goal/Strategy | Expected Outcome the of Project |
|---------|--|---|
| ENV 3.1 | Require dedicated open space for new developments and for changes in land use where appropriate. | Significant Open Space is proposed in this development. |
| ENV 3.5 | Require and enforce maintenance of open spaces. | The applicant will be responsible for maintenance of open space. These areas will be under the development's ownership or an affiliate and will be maintained at no cost to the general taxpayer. |

6.2.4 Stormwater Management Goals & Strategies

| Number | Goal/Strategy | Expected Outcome of the Project |
|--------|---------------|---------------------------------|
|--------|---------------|---------------------------------|

| | | |
|---------------|--|--|
| IS 1.4 | Protect all watersheds within the Town's corporate limits, recognizing that they are the ultimate source of drinking water for Christiansburg and other downstream public water systems. | The project will meet or exceed all current Town standards and specifications for stormwater management at the time of design and development. |
| IS 4.2 | Maintain standards and specifications for design and construction of stormwater management infrastructure. | The project will meet or exceed all current Town standards and specifications for stormwater management at the time of design and development. |

6.2.5 Community Character Goals & Strategies

| Number | Goal/Strategy | Expected Outcome of the Project |
|----------------|--|---|
| CED 8.6 | Ensure that new residential neighborhoods in Town are compatible and integrated with the existing community character. | The apartment use is compatible with surrounding uses. There are existing townhomes and various commercial uses adjacent to or near the project site. |

6.2.6 Housing Goals & Strategies

| Number | Goal/Strategy | Expected Outcome of the Project |
|-----------------|--|--|
| LUP 1.5 | Encourage a wide range of housing types and choices within the neighborhoods including accommodations for those with special needs. | The proposed development will provide more housing options for the Town. Accessibility parking is being provided and there will be 8 first floor units that will support special needs housing as required by building code. |
| LUP 1.5A | Promote housing with universal design features to support aging in place. | First floor apartments are more compatible with aging in place. |
| LUP 1.6A | Strive to ensure new housing developments and infill structures are designed in context with existing built neighborhoods, complement the overall character and architecture of the neighborhood, and do not overtax existing public infrastructure. | The proposed development will be compatible with the adjacent neighborhoods to the east and provides a transition of land uses from Main Street to CMS. |

6.2.7 Existing & Future Land Use & Planning Goals & Strategies

| Number | Goal/Strategy | Expected Outcome of the Project |
|-----------------|--|---|
| LUP 2.2A | Ensure development is compatible with surrounding uses through buffers and | The proposed development will be compatible with the adjacent neighborhood. |

| | | |
|-----------------|--|---|
| | other techniques. | |
| LUP 2.6 | Limit development in environmentally sensitive areas. | There are no known environmentally sensitive areas on the project site. |
| LUP 2.7 | Promote environmentally friendly neighborhoods and housing developments. | The proposed design will promote a friendly neighborhood. |
| LUP 2.8 | Ensure land use decisions are integrated with transportation decisions to maintain or improve access to various uses and prevent future gridlock. | The proposed new public road connecting Moose Drive and Bishops Gate Road will provide cross connectivity of subdivisions as will the parking lot connection to the Harkrader Townhome project. |
| LUP 2.8A | Promote connected and walkable neighborhoods. Promote multiple entrances, interconnected streets, and pedestrian sidewalks or trail linkages for neighborhood development. | A sidewalk is proposed for the development. The site is within walking distance of downtown, which will support the desired walkability of the area. |
| LUP 2.8B | Effectively manage traffic and encourage attractive and safe pedestrian-friendly environments through landscaping and traffic calming features. | A sidewalk is proposed for the development. The site is within walking distance of downtown, which will support the desired walkability of the area. |
| LUP 2.12 | Promote new infill development and redevelopment utilizing existing infrastructure. | This is an infill development adjacent to downtown. |

A. Appendices

SHEET 1: Vicinity Map

SHEET 2: Existing Conditions

SHEET 3: Master Plan

SHEET 4: Elevations



**BALZER
& ASSOCIATES**
a Westwood Company

Roanoke / Richmond
New River Valley
Shenandoah Valley

balzer.cc | westwoodps.com

80 College Street
Suite H
Christiansburg, VA 24073
540.381.4290

PRELIMINARY - NOT FOR CONSTRUCTION

CHRISMAN & WEST MAIN ST
PLANNED HOUSING DEVELOPMENT
EXISTING CONDITIONS

RIVER MAGISTERIAL DISTRICT
TOWN OF CHRISTIANSBURG, VA 24073

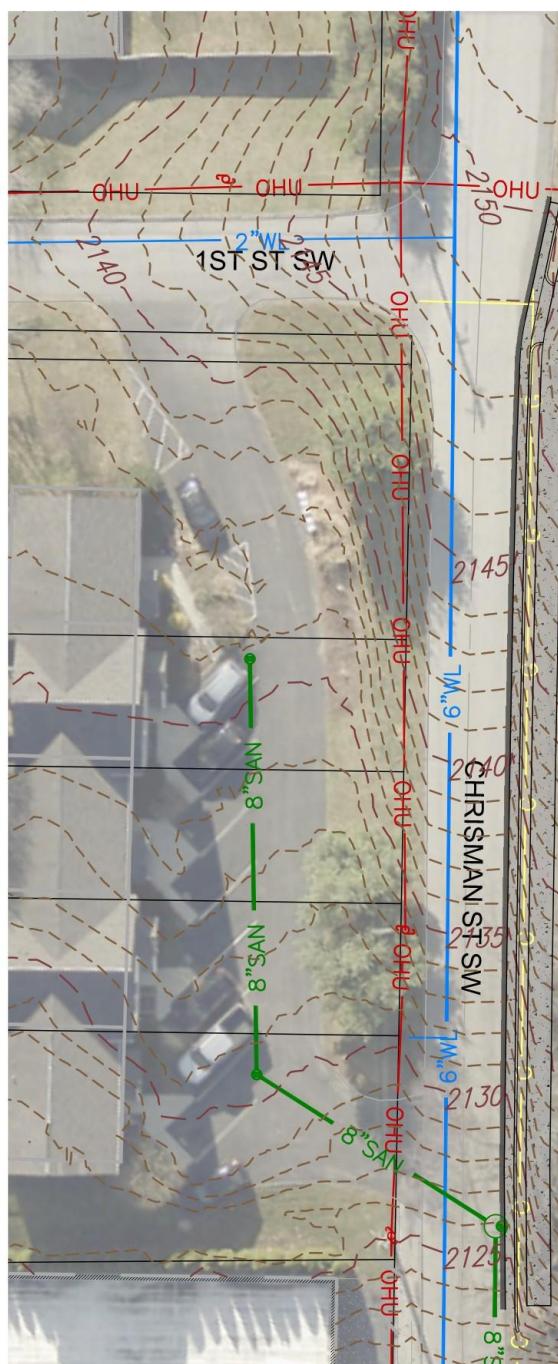
DRAWN BY LMK
CHECKED BY JRT
DATE 9/22/2025
SCALE 1" = 40'
REVISIONS

Z2

PROJECT NO R0063052.00



DATA SOURCE: LOCAL GOVT GIS, FEMA, FWS, USDA, USGS, VDEM, VDOT.
CONCEPT PLAN NOTE: THIS PLAN IS FOR CONCEPTUAL PLANNING PURPOSES AND HAS BEEN PREPARED USING COMPILED INFORMATION. A CURRENT FIELD SURVEY HAS NOT BEEN PERFORMED TO VERIFY ALL EXISTING CONDITIONS ON-SITE.



ZONING SUMMARY:

BASE DISTRICT R-3 WITH PLANNED HOUSING DEVELOPMENT:
TOTAL AREA= 2.03 AC (EXISTING SITE)

MULTI-FAMILY:
ALLOWABLE DENSITY: 20 UNITS PER AC
PROPOSED DWELLING UNITS: 28 UNITS
PROPOSED DENSITY: 13.79 UNITS PER AC

MINIMUM LOT AREA: 15,625 SF
MAXIMUM HEIGHT: 45' FROM GRADE AT THE FRONT OF THE STRUCTURE

PARKING:
REQUIRED: 56 SPACES (2 SPACES/DWELLING UNIT)
PROVIDED: 56 SPACES

SETBACKS & YARDS

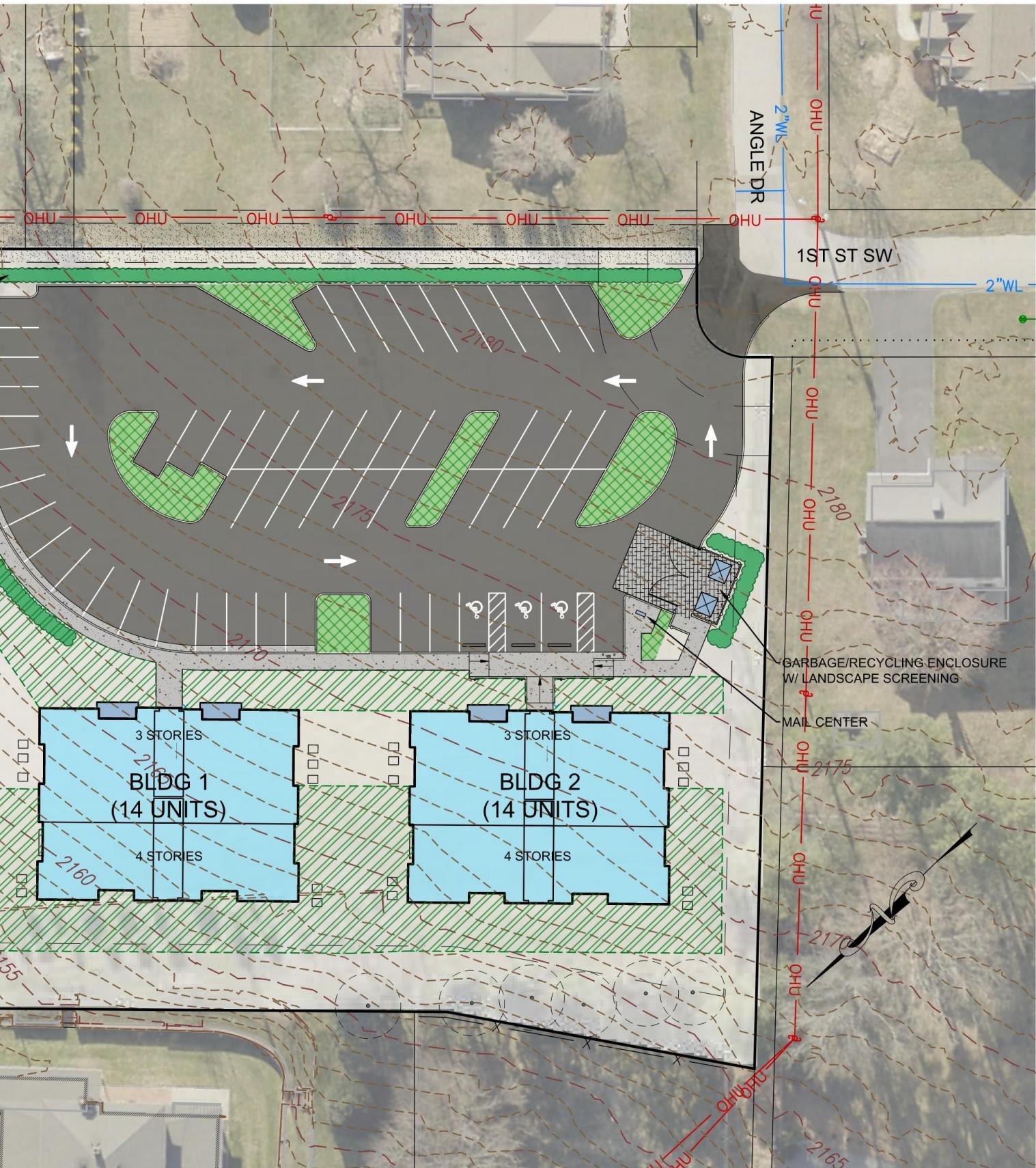
FRONT SETBACK: 30' FROM RIGHT OF WAY
SIDE SETBACK: 10'
SPACE BETWEEN BUILDINGS: 25'
REAR SETBACK: 20'
PARKING SETBACK: 15' FROM RIGHT OF WAY

PARKING LOT GREENSPACE

REQUIRED: 2,240 SF (40 SF/PARKING SPACE)
PROVIDED: 2,302 SF

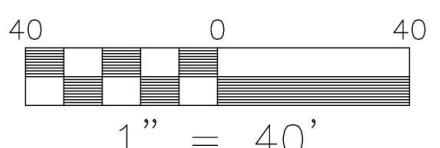
OPEN SPACE:

REQUIRED: 11,200 SF (400 SF/DWELLING UNIT)
PROVIDED: 12,353 SF



LEGEND:

- PARKING LOT GREENSPACE
- OPEN SPACE



CHRISMAN & WEST MAIN ST PLANNED HOUSING DEVELOPMENT MASTER PLAN

RIVER MAGISTERIAL DISTRICT
TOWN OF CHRISTIANSBURG, VA 24073

PRELIMINARY - NOT FOR CONSTRUCTION



Roanoke / Richmond
New River Valley
Shenandoah Valley

balzer.cc | westwoodps.com
80 College Street
Suite H
Christiansburg, VA 24073
540.381.4290

Z3

DRAWN BY LMK
CHECKED BY JRT
DATE 9/22/2025
SCALE 1" = 40'
REVISIONS

PROJECT NO R0063052.00



BALZER
& ASSOCIATES
a Westwood company

Roanoke / Richmond
Shenandoah Valley
New River Valley

balzer.cc | westwoodps.com

80 College Street, Suite H
Christiansburg, VA 24073
540.381.4290



CHRISMAN APARTMENTS
NEW CONSTRUCTION
REAR ELEVATION

CHRISTIAN STREET SW
(ANGLE DR. & ST ST. SW)
CHRISTIANSBURG, VIRGINIA

DRAWN BY: STC/IMC
DESIGNED BY: LMC
CHECKED BY: ARW
DATE: 03/31/2025
SCALE: 1/4" = 1'-0"
REVISIONS: 1 08/29/2025 3RD PARTY
COMMENTS/ADDITIONAL STORY

REAR ELEVATION

(A2.03)

1/4" = 1'-0"

PROPOSED DOWNSPOUT LOCATION ATTACH TO
BUILDING FACE PER MANUFACTURER'S
SPECIFICATIONS IN AT LEAST THREE LOCATIONS.
CONNECT DOWNSPOUT TO SUB-SURFACE DRAINAGE
SYSTEM, OR SURFACE BLOCKS PER CIVIL DRAWINGS.
TOP OF BOOT CONNECTION SHALL BE AT LEAST 6"
ABOVE FINAL GRADE OR ADJACENT SIDEWALKS

BRICK, COLOR TO BE SELECTED FROM
MANUFACTURER'S FULL RANGE OF STANDARD
COLORS, SEE WALL SECTIONS

BRICK ROWLOCK, TYPICAL
CORNER BOARD AND
FRIEZE, TYPICAL

(A3.06)

1

A3.06

30'-7 1/2"

BRICK LEDGE

1

A4.01

GUARDRAIL, CONSULT OWNER FOR
MATERIAL AND STYLE, TYPICAL

1

A4.01

ROOF SHINGLES OVER ROOF
FELT, TYP.

RAKE BOARD, TYPICAL

HORIZONTAL SIDING
COLOR 1, TYPICAL

HORIZONTAL SIDING
COLOR 2, TYPICAL

HORIZONTAL SIDING
COLOR 3, TYPICAL

RIDGE VENTS AS
REQUIRED, TYPICAL

1

A3.05

1

A3.05

T.O. WALL THIRD FLOOR
+30'-1 1/2"

T.O. SUBFLOOR THIRD FLOOR
+21'-1 1/2"

T.O. WALL SECOND FLOOR
+19'-6 3/4"

HORIZONTAL SIDING
COLOR 1, TYPICAL

T.O. SUBFLOOR SECOND FLOOR
+10'-6 3/4"

T.O. WALL FIRST FLOOR
+9'-0"

T.O. SUBFLOOR FIRST FLOOR
+0'-0"

T.O. BASEMENT
-1'-6 3/4"

10'-6 3/4"

9'-0"

10'-6 3/4"

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Jillian Layton

From: Reuel Edson <reuelaedson@gmail.com>
Sent: Tuesday, December 2, 2025 4:33 PM
To: PlanningCommission; jknies@christiansburg.org
Subject: Chrisman & West Main Street Development
Attachments: 02292 Chrisman and West Main Townhomes layout 23 0711.pdf

Good Afternoon Commission and Chair,

My name is Reuel Edson, I live at 101 Angle Drive.

Thank you for hearing the concerns of the community last night. I wanted to follow up with two items that I think will be helpful.

1. Access to Chrisman
 1. I have attached the initial development design that was submitted in July 2023 from the "Plans in Review" page. It shows access to both Angle/First and Chrisman.
 2. Per the gentleman from Balzer & Assocs. access to Chrisman was prohibited due to steepness of the grade. That doesn't appear to be the case based on this initial submission
2. Student Demographics
 1. I'm not opposed to students in my neighborhood. My concern is how the demands from the university population drive community development needs.
 2. Chair Knies's point is well taken that VT has done well given recent declines in enrollment nationwide from 2010-2021. However, this does not address the upcoming ["demographic cliff"](#).

Thank you for your time consideration.

Reuel Edson

Jillian Layton

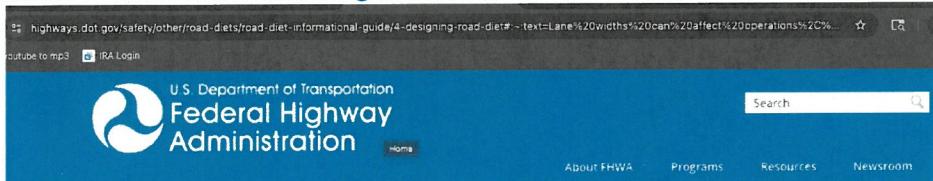
From: David O <domiecin@gmail.com>
Sent: Wednesday, December 3, 2025 8:31 PM
To: Devon Shields
Cc: Jillian Layton; Dirut
Subject: Public Comment - CUP-2025-06 Chrisman and West Main Street Planned Housing Development

Hi Devon and Jillian,

I'm Diane's Son-In-Law and I wanted to reach out to you to give some more information regarding this planned housing development since I wasn't able to attend the meeting in person. I'm not sure how wide the developers said Angle Drive or First Street are, but there is only 13 feet of paved road. The average vehicle is 6-7 feet wide. Currently whenever two people come up to each other down these roads someone has to pull over into the grass to allow the other driver by. I did some research, and the size of Angle Dr and First Street are actually only that of a single lane road. They really should be marked as One-Way roads to begin with.

Per the US DOT, a single lane should be 10-12 feet wide for town/village neighborhoods

(<https://highways.dot.gov/safety/other/road-diets/road-diet-informational-guide/4-designing-road-diet#:~:text=Lane%20widths%20can%20affect%20operations%2C%20safety%2C%20and,Includes%20lane%20lines%2C%20edge%20lines%2C%20and%20TWLTLs>):



| Rural | Suburban Neighborhood | Suburban Corridor | Suburban Center | Town/Village Neighborhood | Town/Village Center | Urban Core |
|------------|--|--|---|---|---|---|
| 11' to 12' | 11' to 12' (14' to 15' outside lane if no shoulder or bike lane) | 11' to 12' (14' to 15' outside lane if no shoulder or bike lane) | 11' to 12' (14' outside lane if no shoulder or bike lane) | 10' to 12' (14' outside lane if no shoulder or bike lane) | 10' to 12' (14' outside lane if no shoulder or bike lane) | 10' to 12' (14' outside lane if no shoulder or bike lane) |
| 8' to 10' | 8' to 10' | 8' to 12' | 4' to 6' (if no parking or bike lane) | 4' to 6' (if no parking or bike lane) | 4' to 6' (if no parking or bike lane) | 4' to 6' (if no parking or bike lane) |
| NA | NA | NA | 8' parallel | 8' parallel; see 7.2 for angled | 8' parallel; see 7.2 for angled | 8' parallel |
| NA | 5' to 6' (if no shoulder) | 6' (if no shoulder) | 5' to 6' | 5' to 6' | 5' to 6' | 5' to 6' |
| 30' to 50' | 25' to 35 | 30' to 50' | 25' to 50' | 15' to 40' | 15' to 40' | 15' to 40' |
| 2 to 6 | 2 to 6 | 4 to 6 | 4 to 6 | 2 to 4 | 2 to 4 | 2 to 6 |

Specifically the Virginia DOT says:

Two-car roads in Virginia typically have a minimum travel lane width of 12 feet, leading to a total width of 24 feet for a two-lane road, plus any shoulders. The Virginia Department of Transportation (VDOT) uses a minimum of 12-foot lanes for most new publicly dedicated streets to ensure safety and accommodate larger vehicles, though older standards may have been narrower.

Personally, it makes more sense for the town to designate the roads as one way roads in order to update to newer safety standards instead of trying to force more traffic down these roads. The roads have no real shoulder, no sidewalks, there is nothing. I mean even if we went by VDOT standards in 2016, the

road would STILL not accommodate 2 vehicles. (https://www.vdot.virginia.gov/x/vdotvirginiagov/doing-business/technical-guidance-and-support/technical-guidance-documents/location-and-design/2016-road-and-bridge-standards/autopublish/section800/803_21.pdf)

Here is a great study by the US Federal DOT about the frequency of accidents and the importance of road width on low-traffic volume roads

(<https://www.fhwa.dot.gov/publications/research/safety/humanfac/94023.cfm>). As you see, the wider the lane width, the less chance of accidents or other issues. These roads again are **single** lane roads if we follow the standards of this research. They specifically call this out in the findings - "Accidents on low-volume roads are affected primarily by roadway width, roadside hazards, roadway terrain, and the number of driveways per mile." With roadway width being the **first** cause for the increase in accidents here. This development will only exacerbate an already existing problem with these roads. Unfortunately, there isn't room to widen the roads another 10-12 feet.

While I agree affordable housing in town is a GREAT idea. It just isn't in the proposed location on Angle Dr & First Street. There are many families in our neighborhood with young children. With this proposed development no child will be able to play outside safely anymore. They won't be able to ride bikes, scooters, or rollerblade.

Not only is the increase in traffic an issue, the proposed development does not provide adequate parking for the amount of people they want to support living in these apartments. So where will they all park? Are they supposed to start parking down Angle Dr or First St? There is no place for them to park on these streets, they will be blocking driveways and filling the entire street due to the small size of the streets.

Again, I'm all for affordable housing in Christiansburg as there is a huge need for this, but it just doesn't logically or logically make sense to put it here as proposed.

Thanks,
David Omiecinski

Jillian Layton

From: DIANE GABRIEL <dirut@aol.com>
Sent: Thursday, December 4, 2025 4:04 PM
To: Jillian Layton
Subject: Fwd: Public Comment - CUP-2025-06 Chrisman and West Main Street Planned Housing Development
Attachments: Public Comment Info.pdf

Sent from my iPhone

Begin forwarded message

From: DIANE GABRIEL <dirut@aol.com>
Date: December 4, 2025 at 3:59:03 PM EST
To: mbarber@christiansburg.org
Subject: Fwd: Public Comment - CUP-2025-06 Chrisman and West Main Street Planned Housing Development

Sent from my iPhone. Mike, apparently they're going to put in some apartment buildings down on Main Street that goes up to Second Street, the street that Roger grew up on. Bethany my daughter lives on Longview and they are planning to reroute traffic through her little neighborhood that does not have the infrastructure for the roads that the traffic is intended for. They really should be one way streets because when two cars go up and down that street one has to pull over and stop. The contractors at the meeting got up there to try to make a very good case for the measurements of the road but Bethany's husband investigated the actual standards for the roads and they do not include the type of things the contractor said at the planning meeting or they left a lot of stuff out. From moral and ethical standpoint, I think it's horrible to displace people that have lived in that little community for so long, have paid taxes, and grew up here like my daughter. She is now raising her own child here and she has to deal with the threat of this horrible traffic and her daughter being unsafe. I understand the need for affordable housing but not of displacement of a long-standing residents who grew up here and pay taxes. Moreover, there's a lot of elderly people there that walk with walkers and canes.

Adding traffic through there is an absolute potential disaster waiting to happen. There are no sidewalks.

I think it is a moral and ethical issue. And I hope that the town Council will see it as such. I know the revenue from the apartments is very seductive. However, there is another human factor going on here from people who have paid taxes for a long time to live in Christiansburg. I'm not denying that there's a need for that type of housing but then they need to consider routing traffic entering down by Main Street and immediately turning into the planned apartment complex. It's a travesty what they're trying to do.

Thanks,
Diane Gabriel

| | | |
|--|---|--|
| CHARLES D BROWN BRENDA J BROWN 100 ANGLE DR CHRISTIANSBURG VA 24073 | REUEL A EDSON KATHERINE P EDSON 101 ANGLE DR CHRISTIANSBURG VA 24073 | BONNIE GILBERT AMBER DANAEE GILBERT 502 W MAIN ST CHRISTIANSBURG VA 24073 |
| NEEL R PARIKH 1551 UNION ST UNIT 4 SAN DIEGO CA 92101-3438 | REBECCA M SLAUGHTER 101 CHRISMAN ST CHRISTIANSBURG VA 24073 | JAMES W SOUTHERN SHARON K SOUTHERN 306 FIRST ST CHRISTIANSBURG VA 24073 |
| MONTGOMERY COUNTY EMERGENCY ASSISTANCE PROGRAM INC PO BOX 6594 CHRISTIANSBURG VA 24068-6594 | MTK PROPERTIES LLC 2869 RINER RD CHRISTIANSBURG VA 24073 | CLAYCO LLC PO BOX 10397 BLACKSBURG VA 24062 |
| MONTGOMERY COUNTY BOARD OF SUPERVISORS 755 ROANOKE ST CHRISTIANSBURG VA 24073 | | |